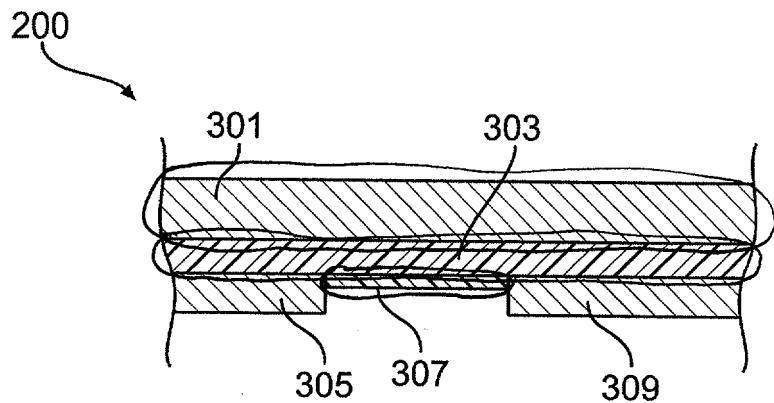


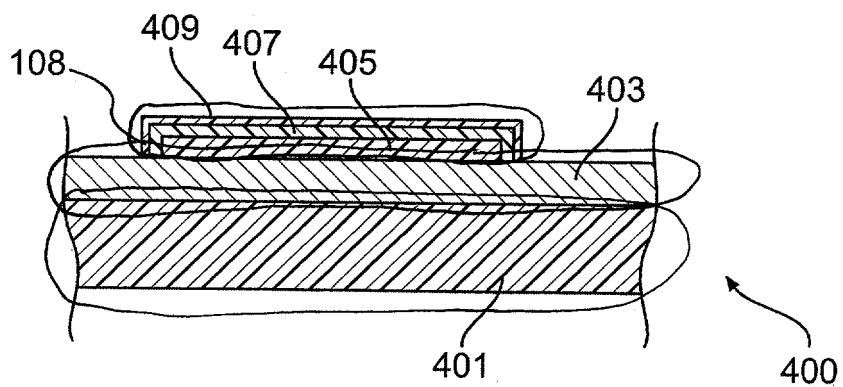


“Annotated Sheet Showing Changes”



STRUCTURE OF A CONVENTIONAL FPC CROSS SECTION

FIG. 3
PRIOR ART



CROSS-SECTION OF THE STRUCTURE OF
A SUSPENSION BONDING PAD

FIG. 4
PRIOR ART



"Annotated Sheet Showing Changes"

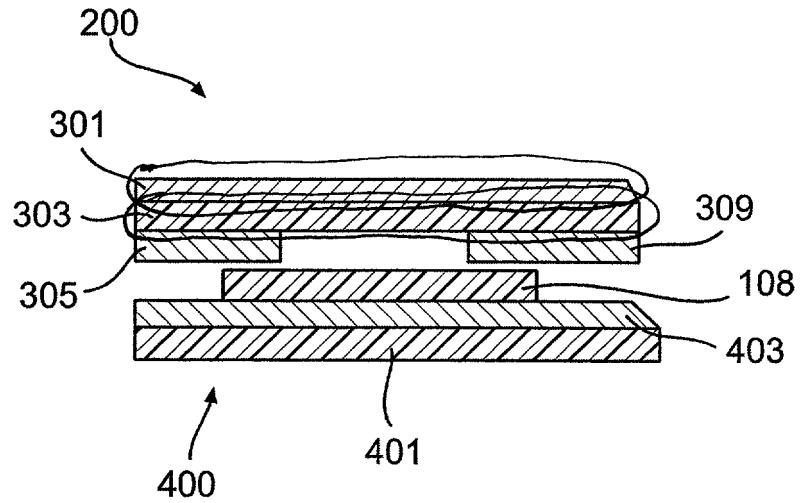
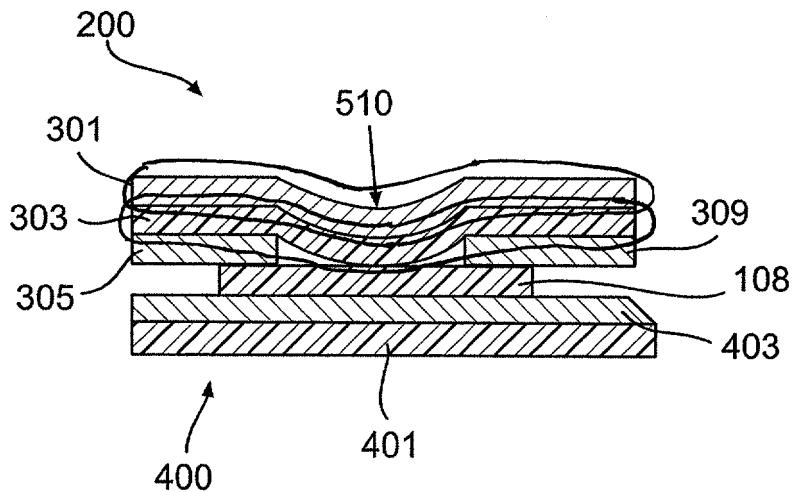


FIG. 5A
PRIOR ART



USING BONDING TOOL AND HEAT PRESS
MAKE THE DEFORMATION FOR CONTACT

FIG. 5B
PRIOR ART



“Annotated Sheet Showing Changes”

AFTER RELIABILITY TEST, WE FOUND
OUT OPEN ISSUE DUE TO SPRING BACK

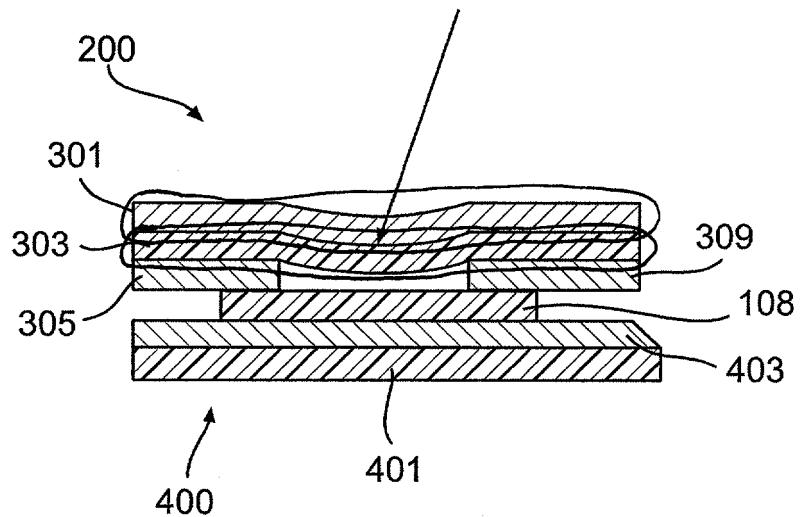


FIG. 5C
PRIOR ART



"Annotated Sheet Showing Changes"

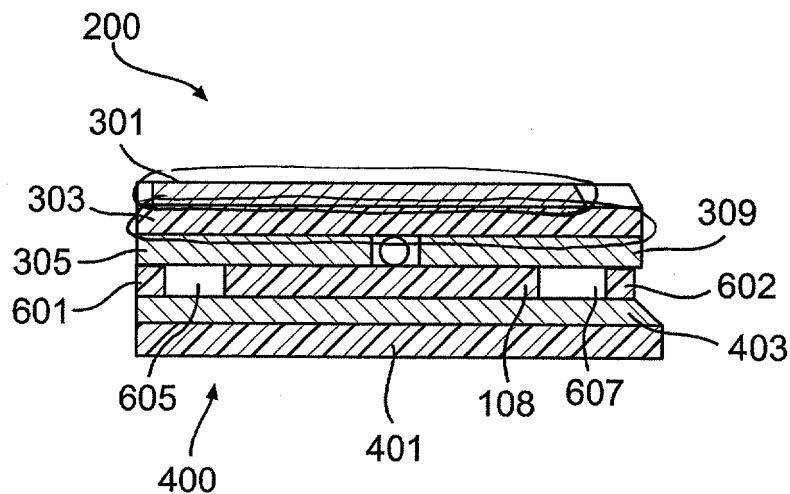


FIG. 6

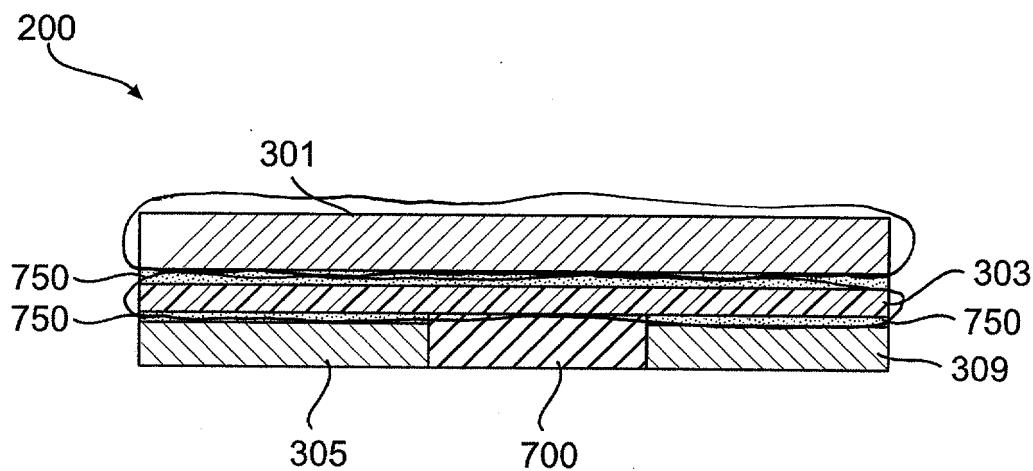


FIG. 7



“Annotated Sheet Showing Changes”

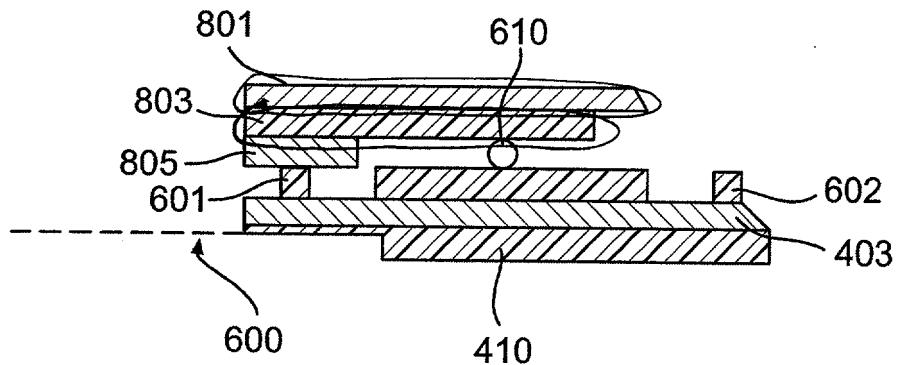


FIG. 8

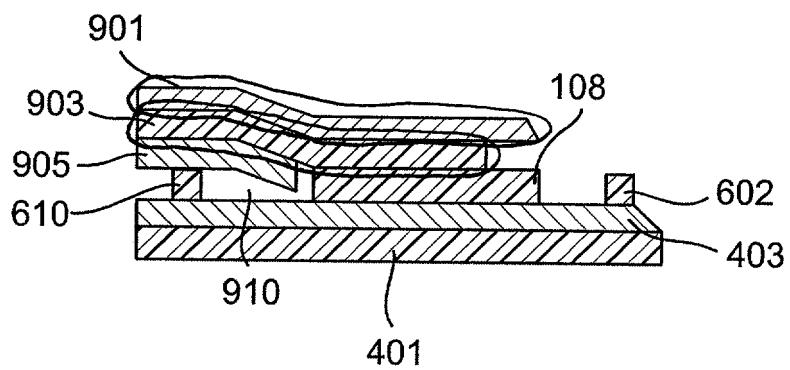


FIG. 9



"Annotated Sheet Showing Changes"

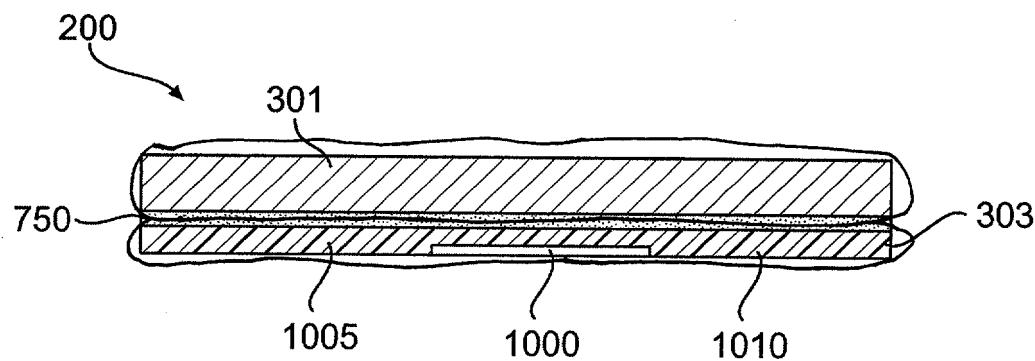
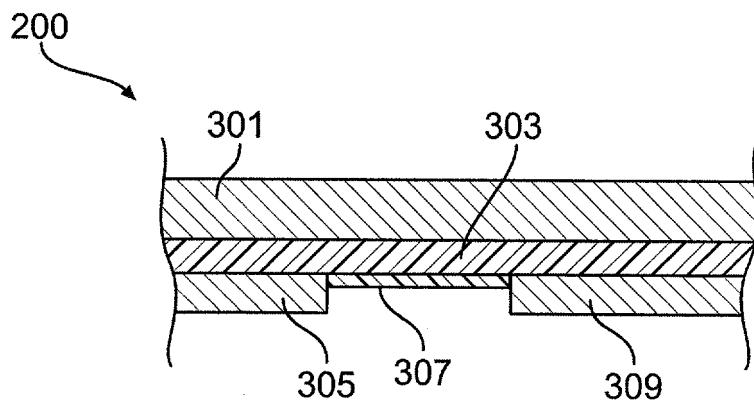


FIG. 10

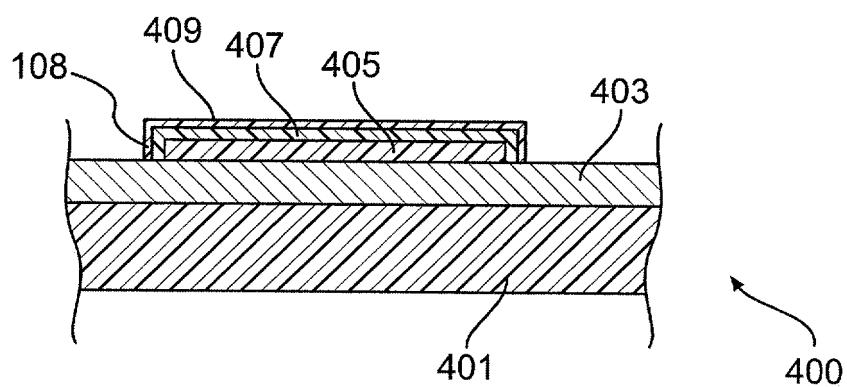


“Replacement Sheet”



STRUCTURE OF A CONVENTIONAL FPC CROSS SECTION

FIG. 3
PRIOR ART



CROSS-SECTION OF THE STRUCTURE OF
A SUSPENSION BONDING PAD

FIG. 4
PRIOR ART



“Replacement Sheet”

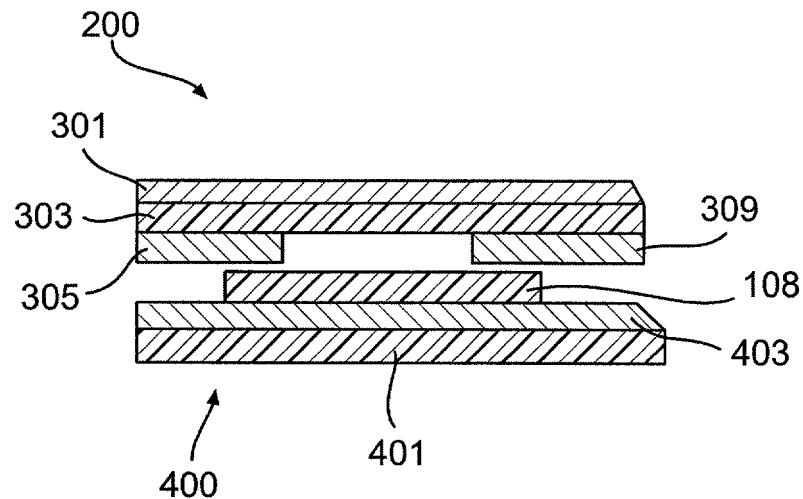
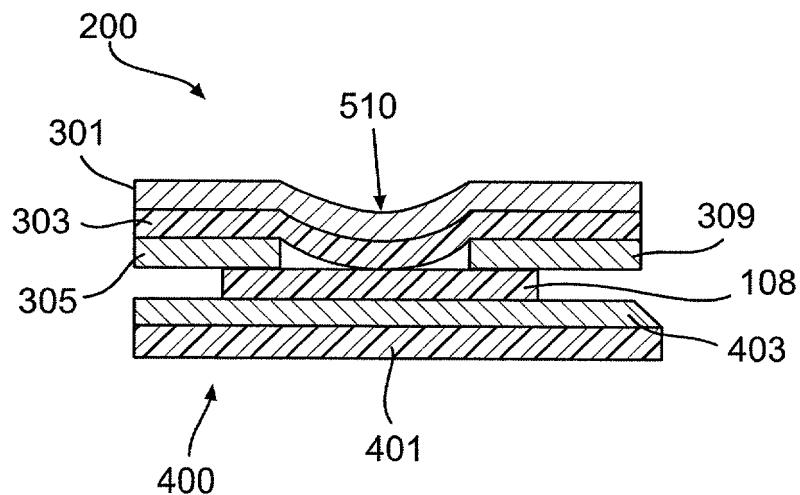


FIG. 5A
PRIOR ART



USING BONDING TOOL AND HEAT PRESS
MAKE THE DEFORMATION FOR CONTACT

FIG. 5B
PRIOR ART



“Replacement Sheet”

AFTER RELIABILITY TEST, WE FOUND
OUT OPEN ISSUE DUE TO SPRING BACK

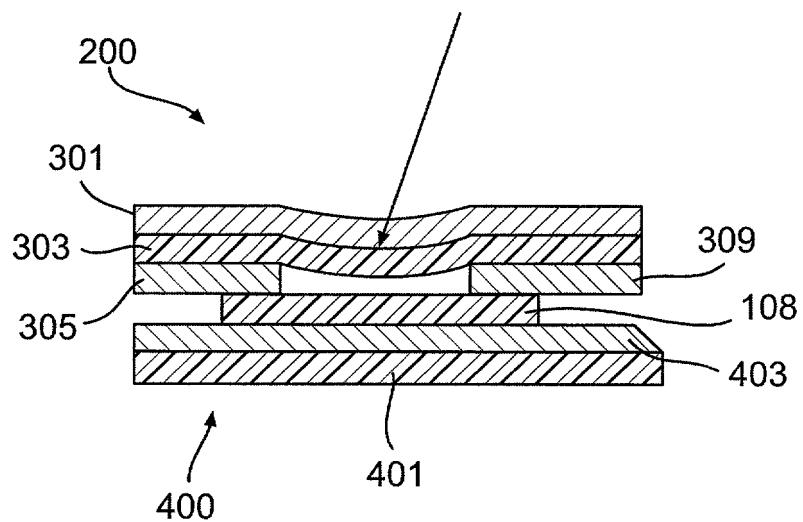


FIG. 5C
PRIOR ART



“Replacement Sheet”

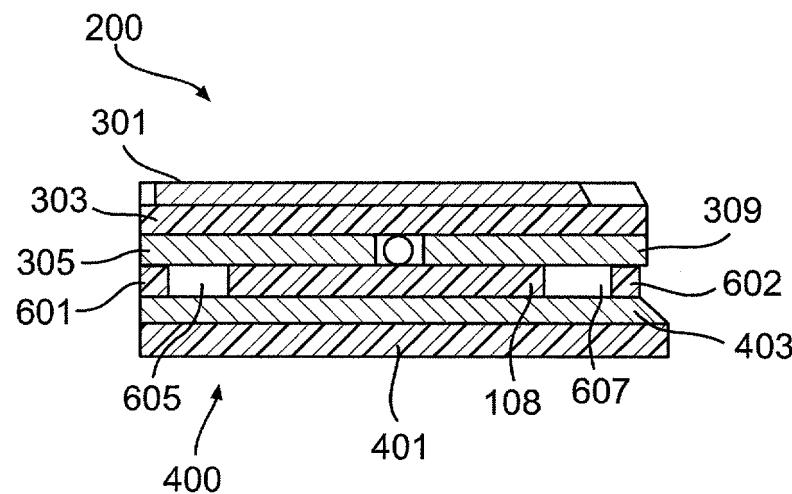


FIG. 6

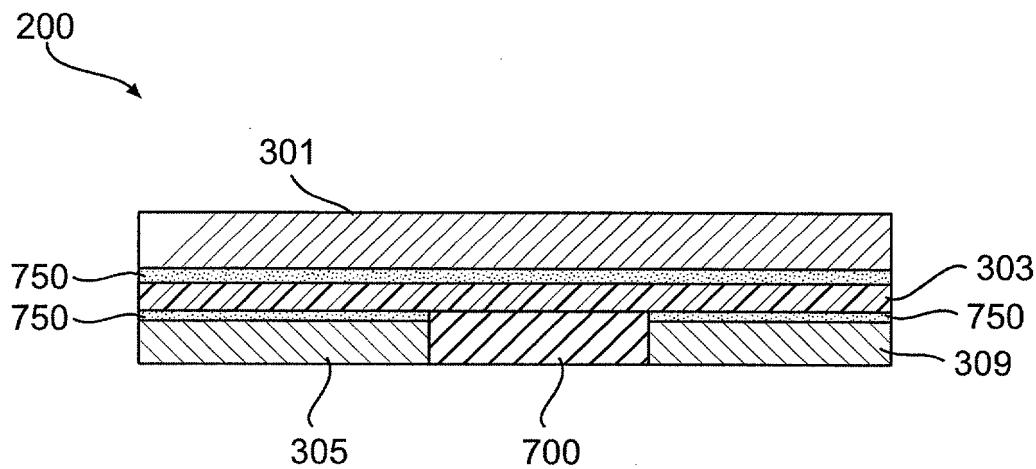


FIG. 7



“Replacement Sheet”

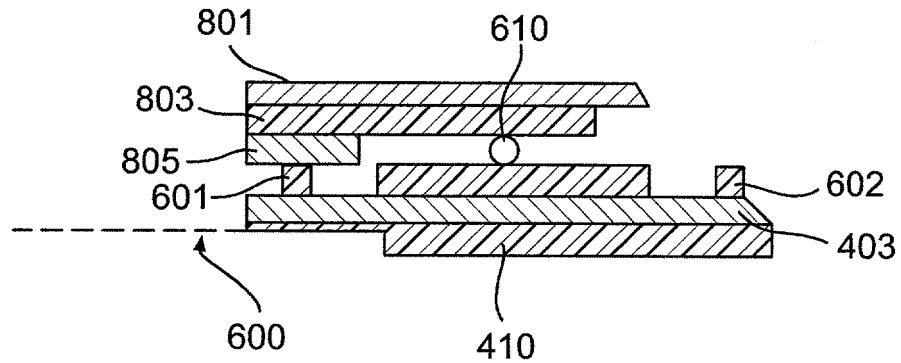


FIG. 8

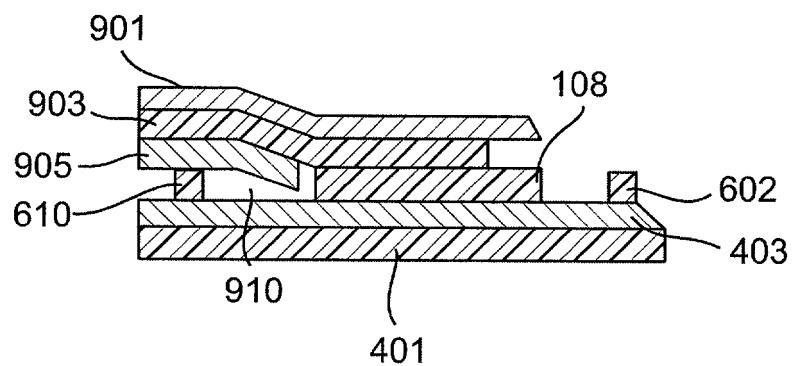


FIG. 9

“Replacement Sheet”

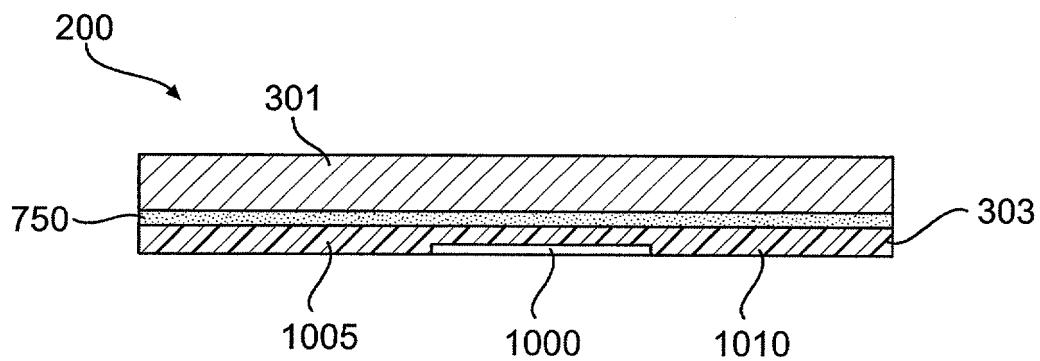


FIG. 10